

**Submission
No 11**

SYDNEY METRO WEST PROJECT

Name: Mr Nathan English
Date Received: 18 September 2023

Dear Committee,

Attached is a powerpoint presentation designed to accompany this submission. This was given to the current NSW Transport Minister, Premier, Treasurer, Housing Minister and Planning Minister sometime prior to this review. For some time now, I have been making the argument for only two new Metro West stations - as a matter of maximising return on a project which in my view, has progressed a break-neck speed with not enough public consultation. The recommendations were first made by me to those listed above when they were in Opposition, a year out from the last election in fact. Despite my encouraging them to operate on the front foot and make additional stations 'an election promise', they told me they'd simply 'keep it on file'.

Since coming to power and announcing this review, the Premier has left the public and the media speculating as to whether this \$25billion project will even proceed. I know it will, as the TBMs have been boring away from White Bay since that very announcement at an average cost of \$5million a day.

Regardless of the review, it appears the Minns Government has only openly consulted in the initial stages with Sydney Metro itself, despite regular updates from myself through email to remind them of the urgency of my proposal - and the progression of the TBMs. I have done this, initially encouraged by the Premier's own statement (two weeks into the job) that he was 'open to more stations' to gain better 'value' for the people of NSW. I agree with this statement, but options to do the most good are already being lost because the TBMs keep boring, with the same alignment unaltered - despite this review taking place.

For me, this is inexplicably incompetent from a Government which claims to have 'done its homework' in relation to the Metro projects and where they are haemorrhaging money on behalf of the NSW taxpayer. The obvious 4km gap between Five Dock and The Bays squeals of the potential for a high-value station in Lilyfield (a site that could interchange with the Inner West Light Rail) - but this new Government has sanctioned the continuation of the TBMs from White Bay along their approved alignment, rather than pause and facilitate a slight alteration in that alignment which could facilitate such a station. The alignment needed to move 400m to the south through Lilyfield west, if a convenient station for light rail to interchange with were to be created. Now, the only station that can be created in Lilyfield is one on the Iron Cove shoreline, near Callan Park, well away from such convenience due to local topographical challenges. I blame the current Transport Minister for this negligence of co-ordinated thinking and inability to pause the project - I gave her full and multiple warnings via email of the opportunities at stake.

As submissions for this enquiry close, the two TBMs from White Bay continue to bore westward and are now leaving Lilyfield, positioned under the waters of Iron Cove. The opportunity to provide those on the Inner West light rail line with a swift and elegant interchange to Metro West (which might also have facilitated decent placemaking and new housing close to the eastern harbour city) has been squandered. A lost opportunity for Sydney's broader Inner West (which would have benefitted from the regular direct connections to Sydney Olympic Park and Parramatta's CBD), and it

baffles me that a large part of this Inner West light rail catchment sits also within Minister Haylen's own electorate.

For me, this is highly regrettable - as it was avoidable. For years I have advocated for a light rail spur (off the existing Inner West light rail line) to Balmain, which could also interchange with The Bays Metro Station near the White Bay Power Station. Sadly, our local Labor politicians squandered that opportunity as well (because it was supported by a Greens MP) and we are now left with only a Rozelle Interchange for motor vehicles, and not so much as a separated cycle way on Victoria Road that could ensure comfortable passage of local people to the future Metro or indeed the closest light rail station they have at Rozelle Bay. It seems active transport connectors to and through major projects are always second fiddle to road and rail themselves - but in this instance, they could be employed to greatly enhance the feed-in catchment to Metro West - especially at The Bays.

Moving on, it is not too late to extend the existing Inner West light rail to White Bay and I'm certain after my many years of campaigning that the Balmain community would be accepting of this (see attached supplementary file). Options still exist, should some minor alterations be made to the current Rozelle Parklands masterplan, or otherwise with some subsequent considerations for a light rail corridor through Stage 2 of The Bays West rezoning efforts in the Rozelle Bay subprecinct. Both should be investigated to ensure the one Metro West station on offer to the Inner West Council area can in fact realistically serve the majority who those who live across the Inner West Council.

At present, the Bays Metro site is difficult to reach on foot if you do not live in Rozelle or Balmain. This will indeed limit the good this one station can do for the Inner West without additional investments. Of highest priority should be the re-opening of the Glebe Island Bridge for active transport (and potentially light rail or electric bus access only) as this will provide new connections from Pyrmont to White Bay and vice versa. Likewise, a visionary investment of a new active bridge linking the end of Glebe Point Road across Rozelle Bay to the Boat House carpark would be transformative. This was suggested in the Bays West Strategy for 2040 (second attached supplementary file) - but in my view, planning should start now so it can coincide with the opening of the Metro West in 2030-31. From the Boat House carpark site, a short pedestrian tunnel could be bored through the Anzac Bridge's sandstone foundations, allowing streamlined access for pedestrians and cyclists using this bridge to not only access Rozelle Bay, but Balmain and the Bays Metro site as well (and vice versa).

It's obvious to me we should have more stations on this Metro West Line than are currently planned. The 20-minute journey time between Parramatta and Sydney CBD was always restrictive and unnecessary. If we are looking to now create more housing, then what we need are more places inbetween that can reach either CBD within 20 minutes - not a journey between the two of 20 minutes! The thing that will always make Metro West more appealing than the existing T1 service, is its all-day frequency for turn-up-and-go services and the far more direct connection being offered into both the the northern Sydney CBD (and its underground interchange that feeds people faster to North Sydney and the rest of the CBD's intensified business areas via the north-

south City Metro) and Parramatta's own CBD, rather than first pulling them through Central and Redfern, the full Inner West and forcing long walks through hilly terrain.

The east Camellia site is one I've been recommending for its ability to appeal to active feed-ins from surrounding growth suburbs that don't have Metro options - and because it's the best place for a midpoint interchange with the Parramatta Light Rail Stage 2. Like Lilyfield west, this would speed up commutes by using LRT as a feed-in and also offering LRT passengers the option of heading also to the Sydney CBD more efficiently - and not just to Parramatta. A metro station in east Camellia (rather than closer to Rose Hill) should be considered as there is little else which will lead to the further neighbourhood investment in cleaning up the former refinery site over the next 50 years. Look to Rhodes and Green Square, a rail station is what it took to clean up such sites for future liveability - nothing less, and Metro West is the line on which to do it for Camellia.

If the Government is cash-strapped to build multiple stations, just one in East Camellia rather than West Camellia (already with light rail coming and arguably more walkable to Parramatta CBD) would be better value over the longer-term for the whole of Greater Parramatta and Olympic Park (GPOP).

Metro West must be utilised as a long-term City Shaping project given its overall cost - it can't be squandered as a momentary indicator of where it will be 'safe' for developers to build housing over the life of the Minns Labor Government. I therefore urge this Government to think big and think long term with this already \$25billion commitment... If the Treasurer believes he can afford more stations in GPOP than one, new bridges across the Parramatta River and Duck Creek would double their catchments for the future. He should also explore a bridge across Rodd Island linking Drummoyne with Lilyfield and consider masterplanning a station there too - even though his Transport Minister has messed up the chance to see this interchange with the existing Inner West light rail...

Excellent accessibility to Metro West stations should be a prominent consideration - access should be possible from every direction - and options to iron-out approaching topography should also be a priority. For example, the Pyrmont Station should have an underground link (like Wynyard Walk) connecting it to both Darling Harbour and the current Fishmarket light rail stop, if not the future Blackwattle Bay redevelopment site beyond Bank Street. This would allow a much easier passage for pedestrians between Darling Harbour and the new Fish Market, as well as transfers to both light rail and Metro from both sites - especially when the weather is not appealing. Likewise, access to the Bays Metro station should feature pedestrian bridges and tunnels that link it to places that are otherwise topographically challenging. Metro stations should all be planned as new mixed-use precincts - with the station as a focus or rallying point for each new community being served.

I agree with the new Government's position that we have to build up not out - but this should be done with lessons learned by the City of Sydney Council which now employ design excellence in all that is built and encourage places for people over traffic, which they also extend to include bicycles.

There should be more secure bicycle lock-ups at every station than there are car spaces. Local buses should be re-routed to feed into Metro West stations - and stations should always interchange with neighbouring existing rail lines with high efficiency, including light rail.

Metro West must interchange with the Inner West light rail somewhere between Lilyfield and Rozelle Bay. People cannot be expected to use it as a feeder into Metro West at Pyrmont when they live more remotely on the LRT line. Such an interchange point (further west) will only expand Metro West's own value - but it will also encourage people to get out of their cars when commuting and use a combination of light rail feeding into Metro West to reach three major employment and cultural centres of the future, ie. Sydney, Parramatta and Olympic Park. This can't be underestimated. Integration is everything, Metro West cannot continue to be treated as a separate standalone transport project to everything else - it has more value as part of the broader network of options.

As a final thought, while I support this project - it is long overdue that the pre-existing Sydney Trains network be upgraded, expanded and given a signalling system worthy of 21st century global cities. This was ignored under the previous Government - Metro should not be treated as the answer or replacement to this, it should be seen as the compliment. The Sydney double-decker suburban system has its merits and we should be proud to continue investing in it.

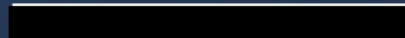
I would be happy to address this review process if needed - to talk about issues of poor co-ordination when it comes to process and what I feel needs to change.

Kind regards,

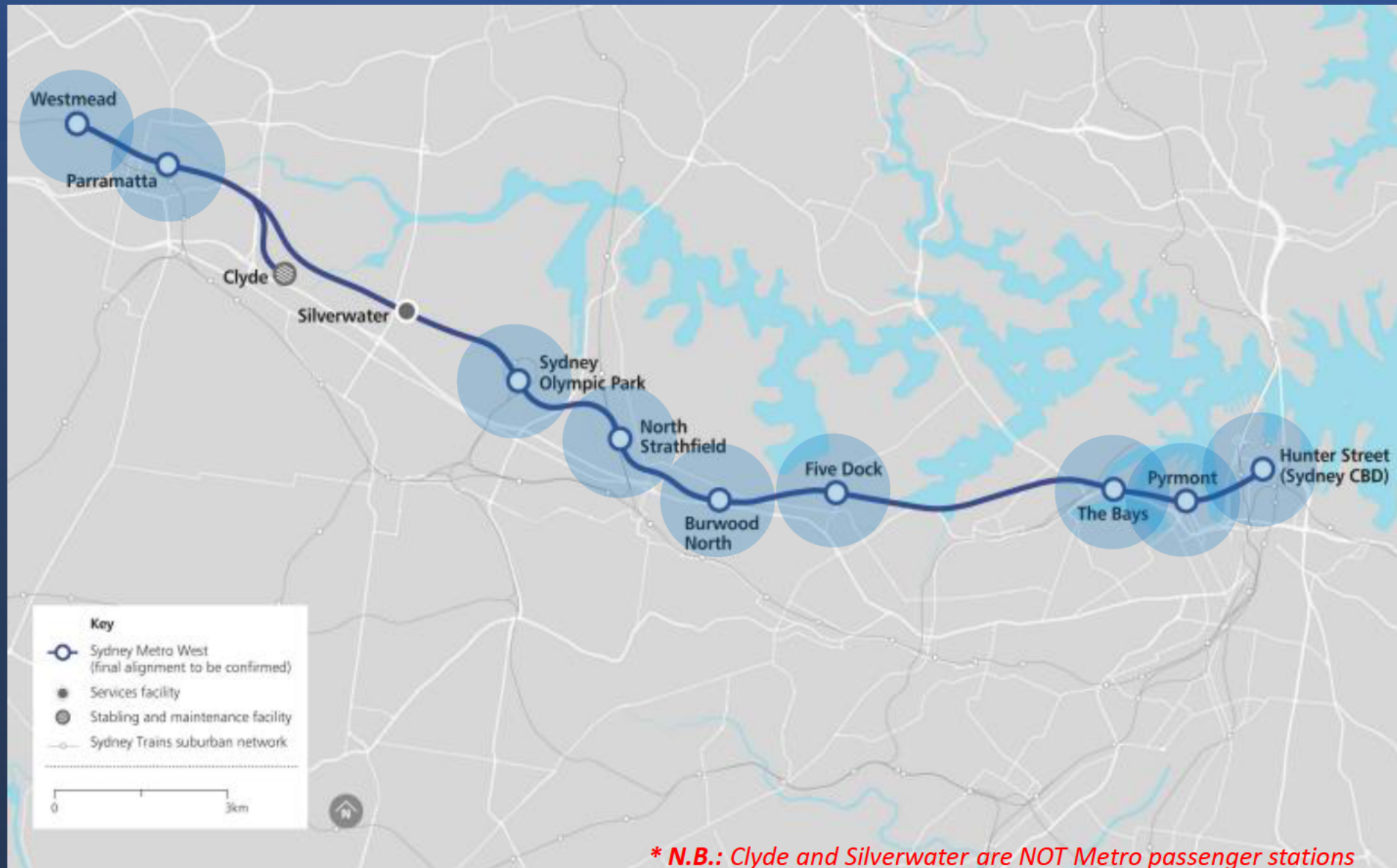
Nathan English,
Lilyfield.

Why Sydney and Parramatta should challenge the Perrottet Government's insistence on a 20-minute maximum travel-time between CBDs for Sydney Metro West...

A city-shaping initiative
by Nathan English, Mplan

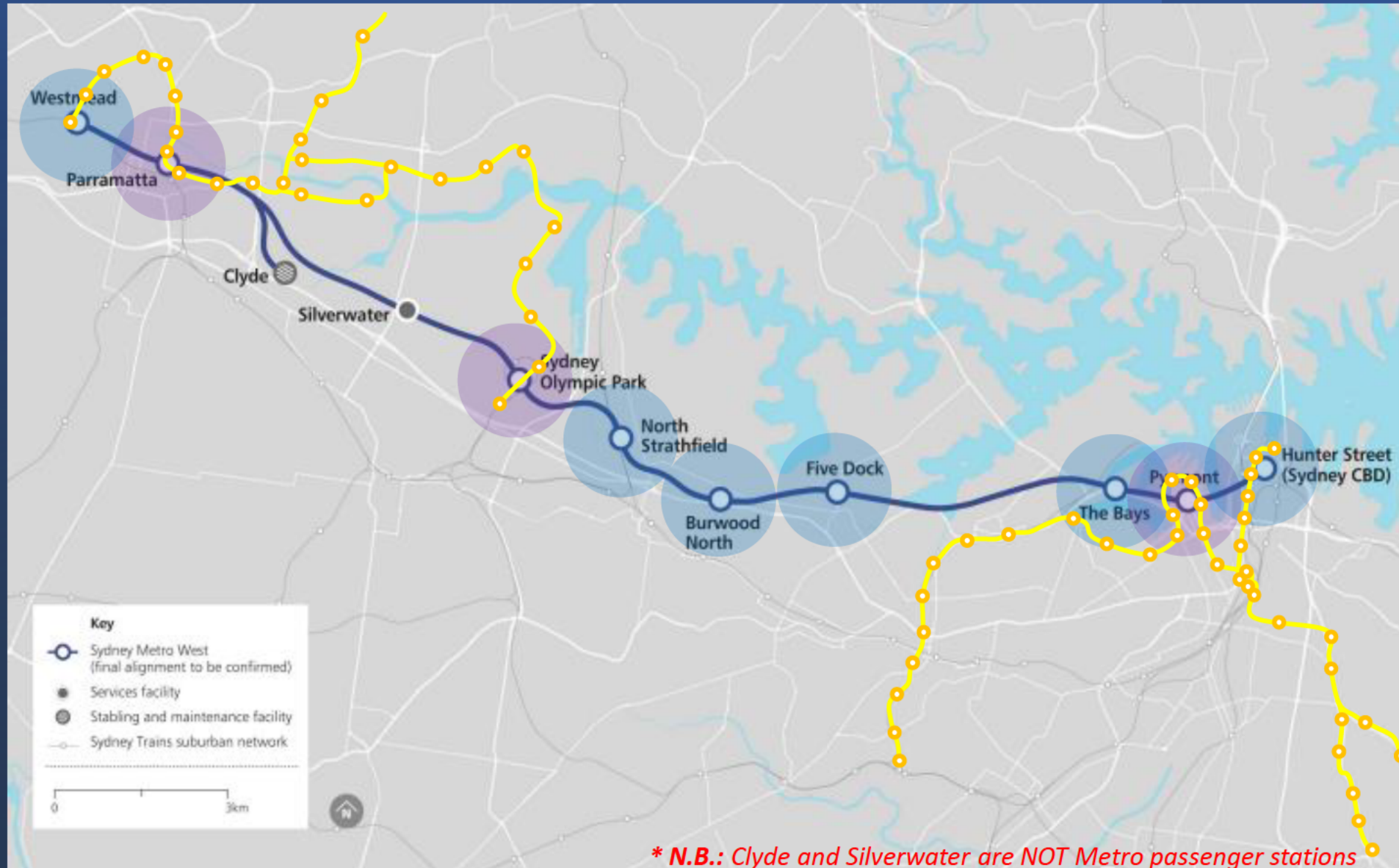


This is Sydney's future Metro West Line (in blue) once complete, relying on only 1-km radial walking catchments for patronage...

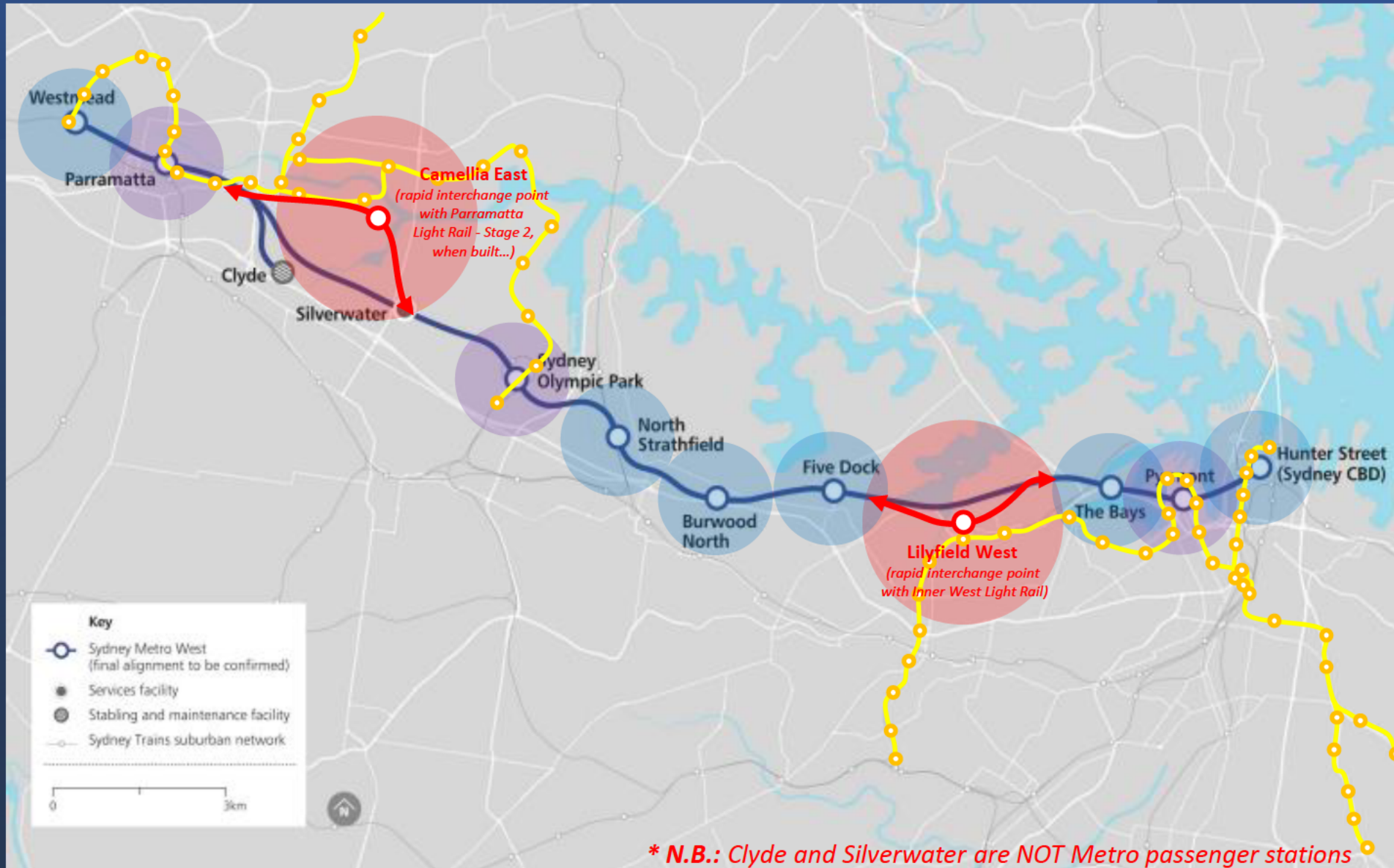


This is Sydney's future Metro West Line once completed (in blue), relying on its 1-km radial walking catchments for patronage...

Clearly there are *two large areas along this corridor with no access to MetroWest services short of taking a long trip by light rail (more than 10 minutes, not including wait times) to a direct transfer node; those being Pyrmont, Sydney Olympic Park and Parramatta...*



And this, is the same Sydney Metro West base map with its 1-km radial walking catchments to planned stations with *two additional* much-needed stations (in red) which could provide 2-km radial *cycling* catchments across predominantly flat topography to bridge those service gaps in the Metro West line and provide faster *interchanges* with light rail to slash overall commuting times.



Two additional stations, with a slight alignment change to the existing Metro West line (to accommodate both) will:

- Provide comprehensive public transport improvements to ALL who live regionally near the Metro West corridor;
- Reduce car dependence and traffic concerns for all neighborhoods which the Metro West line passes through;
- Offer practical toll relief in the form of a truly efficient and accessible public transport alternative, which is time competitive;
- Enhance the appeal of future light rail networks by allowing their users to transfer much sooner to Metro, and gain faster connections to not just one but *two* CBDs in the same efficient timeframe or faster;
- Increase land values for developers;
- Take pressure off the need to provide comprehensive local amenities outside of improved active transport links;
- Support the idea of one continuous harbour foreshore walk between two CBDs;
- Promote better investment in placemaking by the NSW State Government;
- Spark further interest by investors around the two additional stations;
- Add a mere four minutes to the overall commute time between Parramatta and Sydney CBDs (ie. 24 minutes in total), and;
- Still provide a maximum 20-minute commute in *either* direction from most mid-line stations, while future-proofing the accessibility of the Metro West service for any additional changes to metropolitan planning within its corridor.

A Proposal for a **Camellia Metro West** station:

Offers efficient publictransport options for the future Camellia Town Centre and those suburbs north and south of the Parramatta River within a 1-2 km radius
Plus provides a truly efficient modal transfer point (buses, light rail, Metro) and the potential for new local centers in both Silverwater north and Rydalmere south that could act as catalysts for greater urban renewal.

Metro's StationCavern

Suggested Station Entry Point

Construction Site (Re-zoned as mixed-use/commercial upon completion)

Active infrastructure (ie. bridge or tunnel) to speed up connections

T

M

L

F

PublicTransportHubs

Proposed New Metro Route

Current Metro West Route

Proposed electric bus route

Beautified streetscape to encourage transit

L

Parramatta Light Rail Stage 1

Parramatta Light Rail Stage 2 (potential but unconfirmed)

Existing Parramatta Light Rail - Stage 1

New bridges into Rydalmere

Beautified streets help link the Metro to key areas

A new pedestrian bridge here would allow people living in Ermington and Melrose Park to access Metro West services

Alternative route for Parramatta Light Rail Stage 2 (Camellia alignment)

CAMELLIA STATION

1 km radius

2 km radius

Likely extent of walking catchment

Likely extent of cycling catchment

An exclusive bridge to facilitate public and active transport use here could allow streamlined access from Newington and Silverwater into Camellia. Buses services could offer direct transfers to Metro, light rail and WSU's Rydalmere campus.

NB: The low-lying flat topography of the surrounding area makes cycling an excellent option for reaching a metro station built in this proposed location

A new ferry stop on the edge of reclaimed swampland that could also become future residential (along with similar holdings in north Silverwater) could offer additional value to this Metro investment

Planned route for Parramatta Light Rail Stage 2 (funding TBC)

A new pedestrian bridge across Duck Creek at Blaxland Street would allow buses running north-south along Silverwater Road to drop passengers within a few hundred meters of the station

George Kendall Riverside Park

Rosehill Nature Reserve

River Marker

Newington

Silverwater

Western Motorway

A44

Pyramid

Sydney

Hill Road

Kronos Hill

Duck Creek

Blaxland Street

Silverwater Road

Nowell Street

John Street

Grand Ave

Parramatta River

Potential reclamation of swampland

CAMELLIA STATION

CAMELLIA STATION

Potential reclamation of swampland

CAMELLIA STATION

48 m

47 m

45 m

43 m

41 m

39 m

37 m

35 m

33 m

31 m

29 m

27 m

25 m

23 m

21 m

19 m

17 m

15 m

13 m

11 m

8 m

6 m

4 m

1 m

-2 m

Lilyfield has been dismissed by Government as offering 'no real value for redevelopment', but a quick look at the surrounding area shows a large corridor of tired residential and retail properties that few locals may object to seeing given an uplift in their zoning, provided it leads to better retail and transport options, new active linkages, improved architecture and affordable housing between Leichhardt and Lilyfield... These options could include developing into the airspace above Leichhardt Bus Depot and City-West Link (with City views) and over the established light rail corridor between Lilyfield Road and the City-West Link...



In summary, approximately 15 hectares of land could be rezoned and redeveloped for mixed-use in West Lilyfield (with potential views of the Sydney CBD or water views over Iron Cove)



N.B. It is worth surveying this particular cluster of residential homes and imagining what they could become if rezoned... They are currently low-rise, and mostly low-value. These street blocks (between Lilyfield Road and the City-West Link) feature some of the most run-down homes in the Inner West...

While the cost of conglomerating these streets into one parcel of land may appear high, redeveloping this area (ie. levelling out the ground between streets and redeveloping the lot with a solid masterplan to overcome the existing divide of the CityWest Link), could prove transformative in revitalizing this part of Lilyfield for all who remain...

Area Tool
Area: 15.37 ha
Perimeter: 2.527 km

1:4,514
0 50 100m

LEGEND

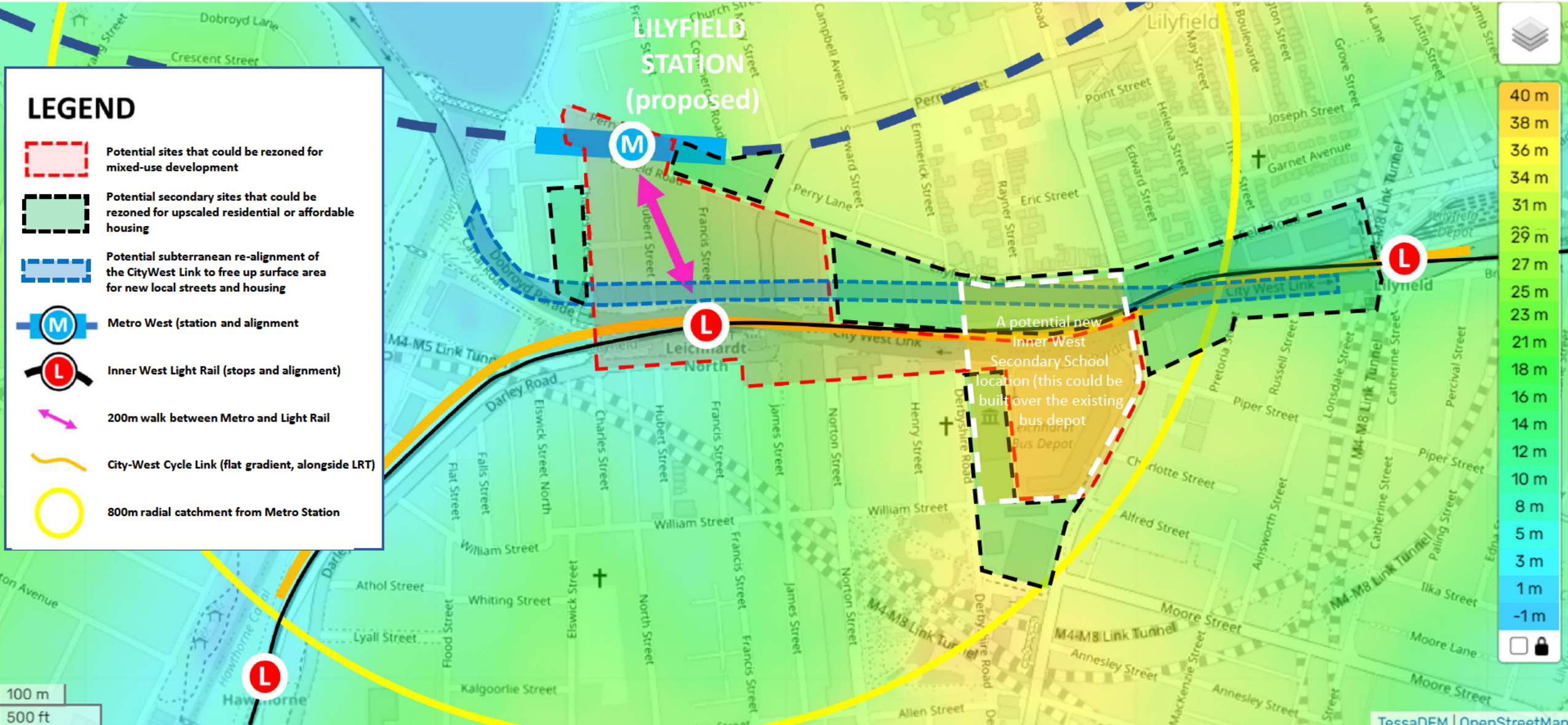
- Potential sites that could be rezoned for mixed-use development
- Potential secondary sites that could be rezoned for upscaled residential or affordable housing
- Potential subterranean re-alignment of the CityWest Link to free up surface area for new local streets and housing
- Metro West (station and alignment)
- Inner West Light Rail (stops and alignment)
- 200m walk between Metro and Light Rail
- City-West Cycle Link (flat gradient, alongside LRT)
- 800m radial catchment from Metro Station

LILYFIELD STATION (proposed)

A potential new Inner West Secondary School location (this could be built over the existing bus depot)

Scale: 0 m, 0 ft

TessaDEM | OpenStreet



In summary, approximately 27 hectares of land could be rezoned and redeveloped for mixed-use within a west Lilyfield Urban Activation Precinct (outlined below) were the CityWest Link realigned underground.



N.B. It is worth surveying this outlined cluster of residential homes surrounding the CityWest Link and imagining what they could become if rezoned as a singular urban activation precinct... Most are currently low-rise and in all honesty, low-value. The established residential streets (between Lilyfield Road and the City-West Link) feature some of the most run-down homes in the Inner West and a challenging topography. That could all be changed by burying the CityWest Link. Likewise, three-storey 1960s red-brick residential flats that lie along the southern edge of the CityWest Link present an opportunity to knock-down and rebuild with enhanced architecture of a higher density. Most carparking could be shifted underground in this area were existing sites redeveloped, allowing the potential for narrowed walkable streets with plenty of trees.

The cost of conglomerating the established lots into larger parcels may seem high, but the appeal of their location (once the City West Link is buried) can't be underestimated. Levelling out the terrain in key places where it proves challenging at present could also enhance walkability around and over the existing light rail corridor. Developing a solid masterplan based on transit-oriented development could also overcome the existing divide created when the CityWest Link was built at a surface level, and could prove transformative in revitalizing this part of Lilyfield for all who remain...

ATT: Submission on the Draft UDLP for the Rozelle Interchange.

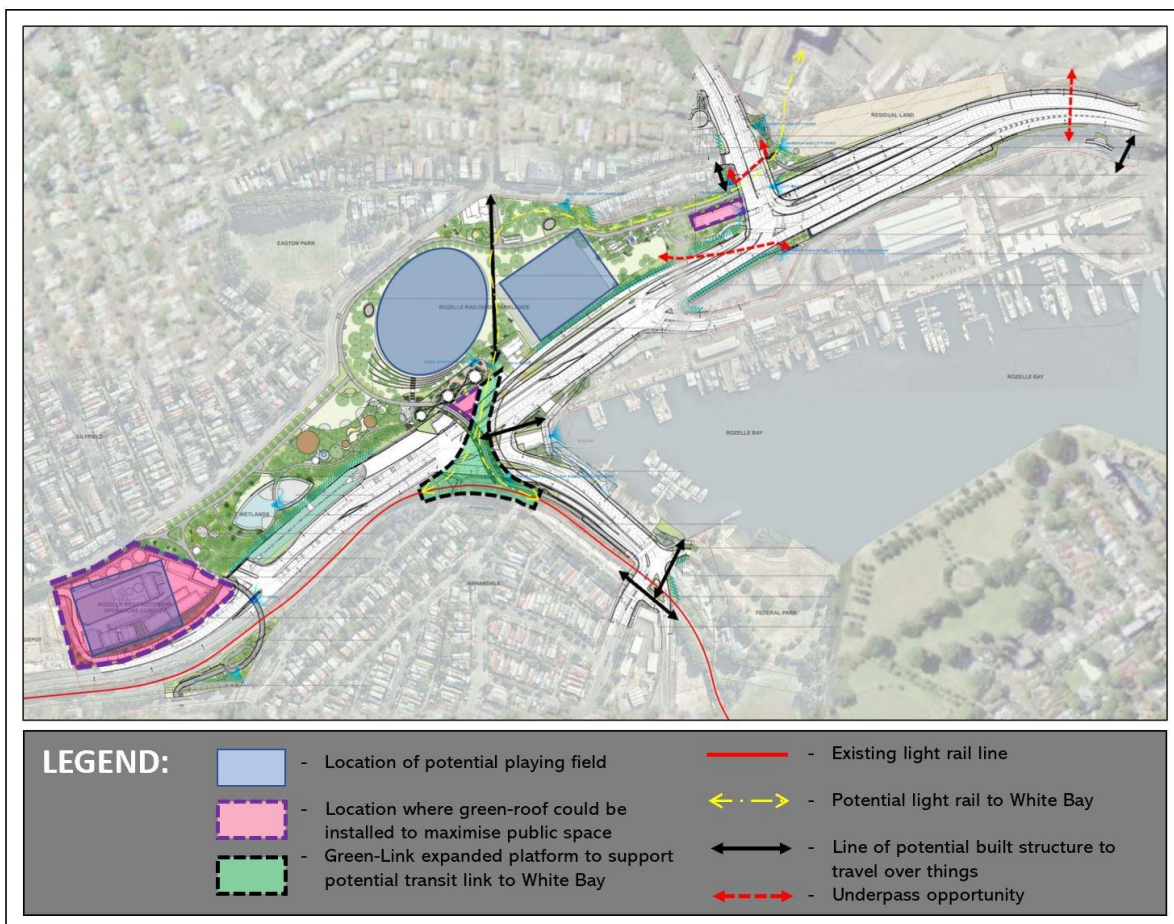
Author: Nathan English MPlan / GradCert (Jour) / BComms (Media)

Lives: Balmain East (knows area thoroughly, former resident of 6 Lilyfield Road)

Dear designers,

Thank you for this opportunity to provide feedback on the Draft Urban Design and Landscape Plan **[the Draft UDLP]** for the Rozelle Interchange.

I would largely approve of your Draft UDLP, but it would be remiss of me as both a local and engaged citizen not to point out a few key elements which I know (as a trained urban planner), would benefit all greatly if you allow for some 'tweaking' of your designs (see below).



Above: A visual summary of all I am proposing, superimposed on the existing draft UDLP.

There are also two critical elements which I feel must be added to the final UDLP, these are:

1. A land reservation on the surface over the park, that can one day host light rail between the existing Rozelle Bay light rail stop and The Bays Metro West Station. This could go a long way to supporting future developments in the

future Bays as well as improve public transport adoption across the Inner West, including the Balmain Peninsula.

2. I personally believe the addition of light rail to The Bays would best be received by my local community, were the designers to decide to employ a bricked-arch viaduct to carry it across the future Rozelle Rail Yards Parkland. This viaduct would not be unlike those which currently support the passage of light rail across Wentworth, Jubilee and Bicentennial Parks to the east of the Rozelle Interchange. These are a form of heritage, much-loved and well-recognised for creating a passive passageway of light rail through interesting green spaces. Their arches are recognised as being adaptable to multiple functions, including men's sheds, art studios, cafes and council storage bays for ground's keepers. The permeable nature of the arches also allows for the free passage of pedestrians and cyclists below the light rail, between different park areas, while maintaining important view lines and a broader sense of space.



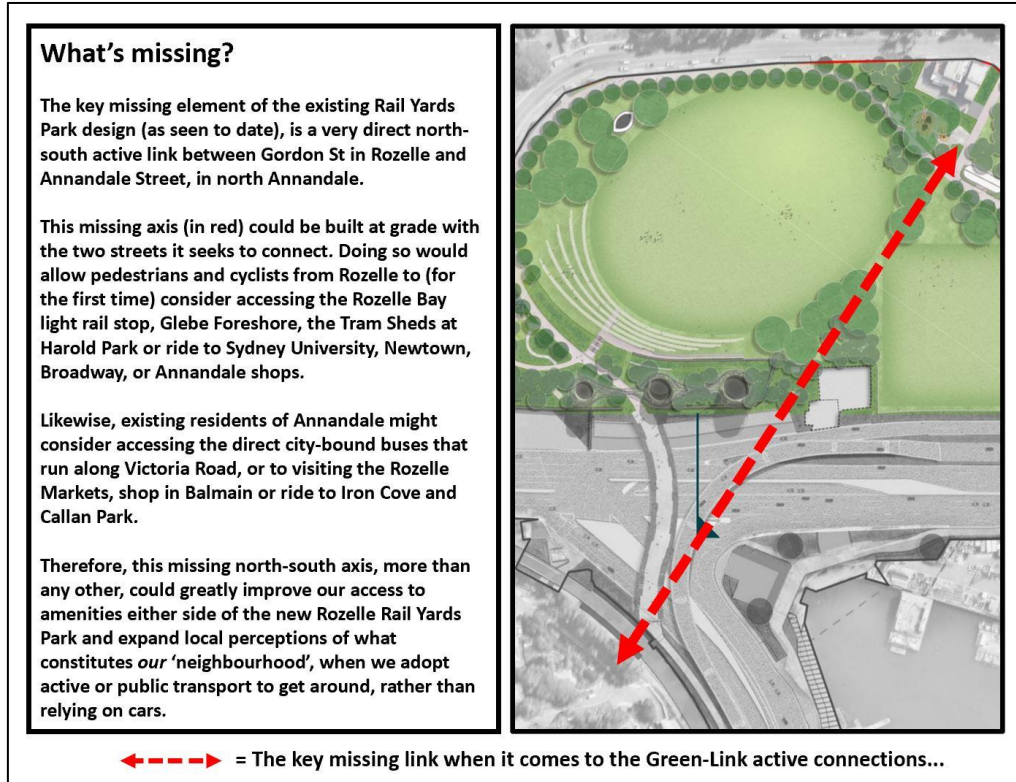
Above: The sort of well-known rail viaduct being advocated in this submission

Other detailed suggestions:

1. There are a number of potential desire lines across the full project, but the most important would have to be a direct north-south corridor with separated paths for active transport between Gordon Street (in Rozelle) and the Rozelle Bay light rail stop (in north Annandale). This would definitely be the *most* obvious missing link between these long-estranged communities on either side of the Rozelle Rail Yards, and it would best link Pritchard Street and Lilyfield Road, as well as the extensive residential networks which lie around both and stem off of them. Such a connection could arguably change the way people move across the CityWest Link – and by which modes.

The fact this link hasn't already been offered serves as a major fault in the current UDLP draft. Designers can anticipate a well-trampled goat trail or 'desire line' to appear as soon as the new park opens, running between the two planned playing fields of the 'village green'. The only way to overcome

this, is to secure the pathway as described. Doing so would mean people in Rozelle on BOTH sides of Victoria Road might actually consider using light rail to the rest of the Inner West – but also, people in Annandale might consider using the direct bus services of Victoria Road given how direct the path would be to reach them (see inset).



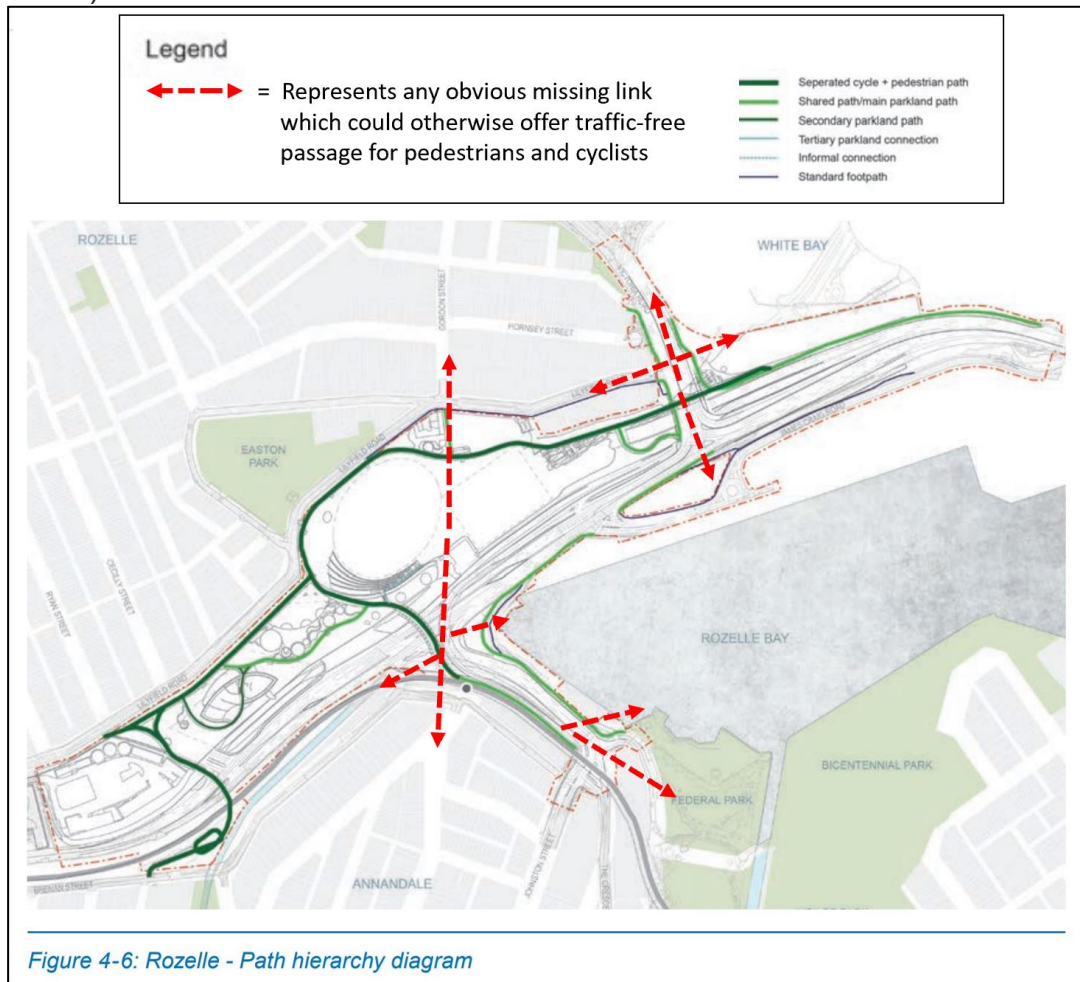
2. There should be no steel mesh walls ruining the views from any of the bridges your are providing. All should be built using glass walls, much like they did to block noise from the tunnel portals at the North Strathfield Interchange (see below).



Above: Noise-proof screening at North Strathfield around WestConnex Fly-ons

These new glass noise barriers are excellent and cut out 95% of traffic noise for pedestrians standing behind them.

3. The draft UDLP is clearly overly reliant on shared paths to provide most of its connecting active corridors (see Figure 4-6: Rozelle - Path hierarchy diagram, below).



Above: Current path hierarchy (in green) and noted missing links for pedestrians and cyclists wishing to avoid any interactions with traffic.

Essentially, all the cycle paths across this project should be separated, lest commuter cyclists will not opt to use this parkland to traverse the area. Instead, they will continue to take their chances (like they have always done) on neighbouring but busy streets like Lilyfield Road... Put simply, the serious cycling fraternity don't like being forced to interact with pedestrians, small children or off-leash dogs when travelling at speed to get somewhere – and this is what shared paths will force them to do.

The conflict this causes (not to mention the way it deters the more vulnerable pedestrians in our society from walking on shared paths, including on the Anzac Bridge) is not worth the anxiety for peds or cyclists. Therefore, a commitment by designers to mostly shared paths can't really be considered a commitment to safe regional cycling on a project of this size. Please separate

the most important pathways across the UDLP for cycling and recreational movement.

4. The New Victoria Road underpass (once completed) must be deep enough to allow for the passage of light rail vehicles between the new Parklands and White Bay. This is so some form of transit can one day operate off-road and interchange with the confirmed Bays Metro West Station on the White Bay foreshore.

Clearance for potential light rail really must be catered for now – as a potential light rail extension has been listed as an *'initiative for investigation in the next 10 to 20 years'* in no less than three of the State's most important and still-current infrastructure strategies. The strategies were all released in 2018 – and no, the confirmed Metro West plan does not supersede that initiative, in fact, it will be complimented by it. Because on this, it remains somewhat baffling (given the takeover of this project by TfNSW) that to date, this project (the Rozelle Interchange) fails to accommodate any reserved corridor on its surface that would allow for such an 'initiative for investigation' to remain cost-effective in future for the taxpayer.

In my view - it is simply not appropriate for TfNSW to build what they have described to me as 'essentially only a motorway project' now, in a way which might obscure or even sabotage the easy passage of light rail in future. Doing so could leave future light rail seeming cost-prohibitive despite what would be an obvious compliment to the region and The Bays, as an extension of the very successful L1 Inner West service.

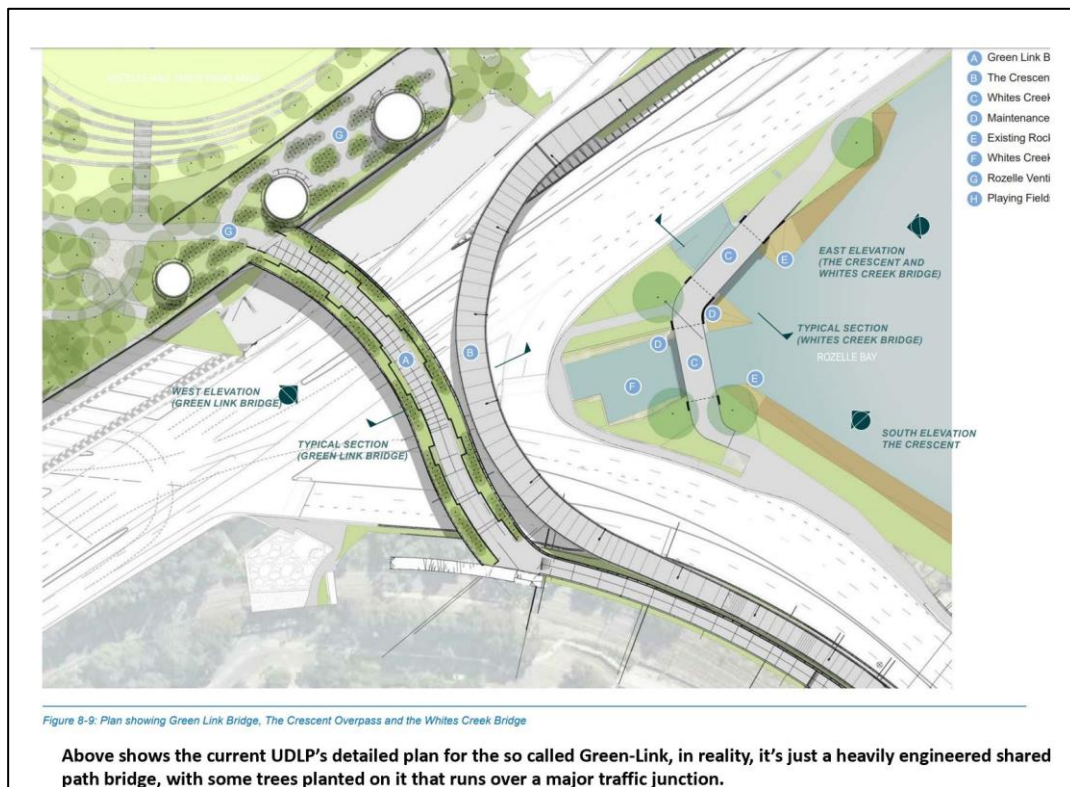
At present, I understand the new underpass (designed by JPB/John Holland) has been planned to be 4.5m in clearance at its highest point – but this slopes away to its south side slightly... If this results in the underpass not being high enough at its southern end for light rail, I imagine future engineers would need to excavate some of the ground below to increase the height of the underpass and thereby achieve the appropriate headroom for LRT vehicles. This would add to costs, lest light rail's passage through the underpass would become very tight indeed. Height is the issue, as it appears there is plenty of width in the underpass planned to accommodate light rail or other modes of transport in an east-west fashion (ie. 15m at its narrowest point and 28m at its widest).

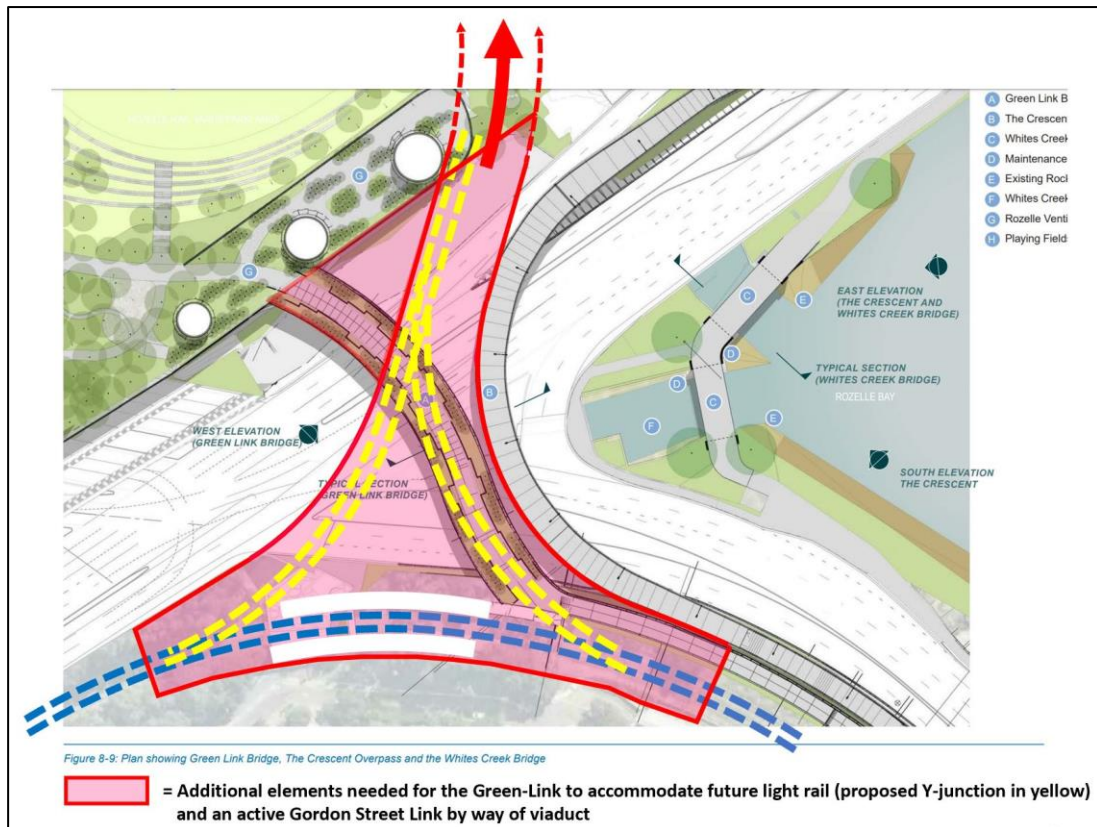
5. Better soundproofing for the park from the surrounding traffic is required. This could be achieved, again, with the glass sound proof walls (I detailed in my second point) lining all Bays Foreshore paths running next to the expanded version of The Crescent. These barriers should also run along the edge of the Rail Park reserve and at the back of houses on the south side of Lilyfield Road (near the future slot motorways) because vegetative screenings simply won't be enough to stop noise pollution. The residents living in that location have already put up with enough.

Designers should also line the edges of the new Victoria Road Bridge (to be built over the Iron Cove motorway portals and the underpass) and along the edge of the Crescent Overpass.

Noise pollution will become one of the key elements which ultimately might ruin this grand public space – if not mitigated. The other will be aerial motor pollution from those still unfiltered stacks...

6. The Crescent Overpass needs to be redesigned as one large base platform over the CityWest, that not only accommodates cars on its fly-over, but the active Green-Link and a potential Y-junction (for light rail) that could feed a future transit link to White Bay. Such an elevated base platform or structure could be built on large stilts also filled with deep soil reserves that double as planter boxes capable of hosting new Port Jackson figs and other largescale vegetation, which would go some way to compensating those we lost in the now erased Buduwan Park. Such an outcome would be far more appealing than the shallow banksia plantings currently depicted along the narrow Green-Link bridge, which really is little more than an elaborate footbridge exposed to traffic noise with some mid-sized trees planted either side.





- I would like to suggest that a future reserved transit corridor should be looking to accommodate two tracks of light rail to White Bay from the existing Rozelle Bay light rail stop. Such a reserved transit corridor would require slightly more space to snake between the two proposed playing fields of the draft UDLP. Any land reservation for transit should be wide enough to accommodate another bricked-arch viaduct, like those the local community is already familiar with in Jubilee Park and Wentworth Park to the east - but this potential new viaduct (for Rozelle Parklands), should probably be designed 6 meters wider than it's predecessors, so that pedestrians and cyclists can also walk along it guaranteeing traffic-free passage alongside light rail, between Gordon Street and the Rozelle Bay light rail stop, not just through its arches below...



Above: What the 'Village Green' will look like as planned in the draft UDLP (eastbound POV).



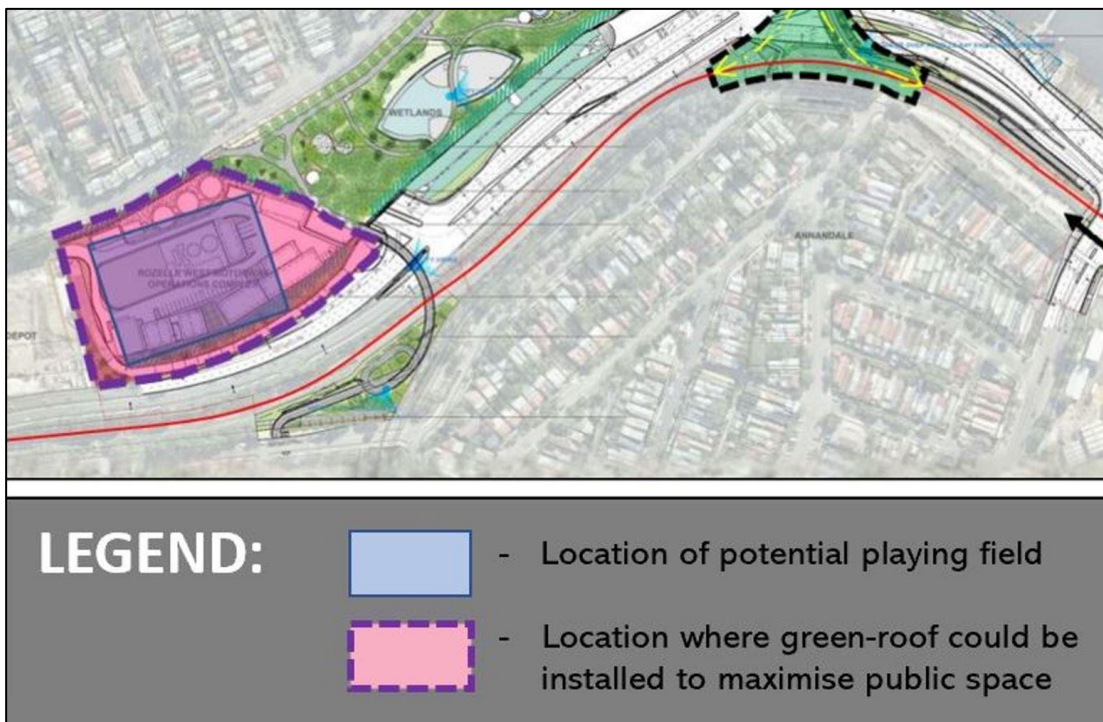
Above: The 'Village Green' could look like this (eastbound POV) with a rail viaduct imposed.

8. Why does the exit portal for the Iron Cove-Anzac Bridge tunnel need to be 'open air' and exposed to the parkland west of Victoria Road? Please cover it over and give more surface space back to park users – it would be an ideal place for some playing courts of a children's playground. This will also reduce traffic noise within the park itself.

Likewise, is it possible to cover the entire motorways operations precinct at the western end of the Railyards Parkland? What I mean, is if it is to exist in a dug-out part of the railyards, why not just hide it under a big green roof? If you did, this area could indeed be large enough to host another full-size football pitch.

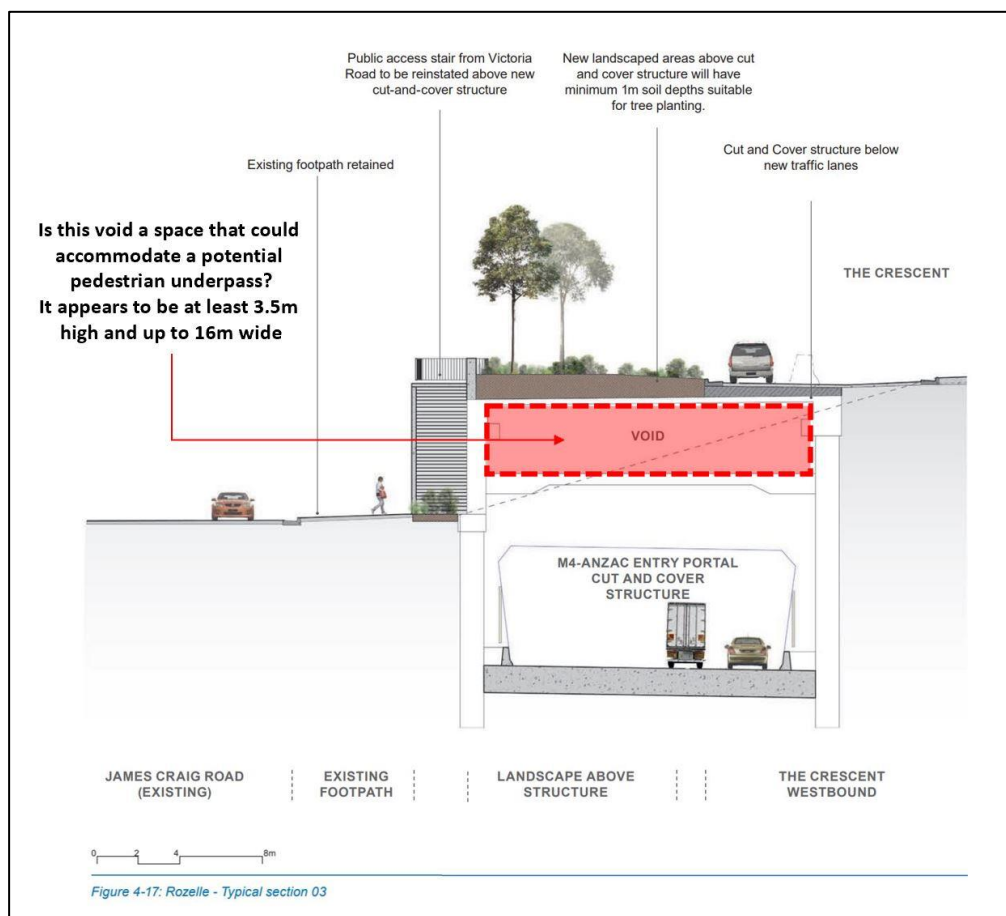


Above: Horizontal beams indicate no passing vehicles will rise above their height, so why not cover them?



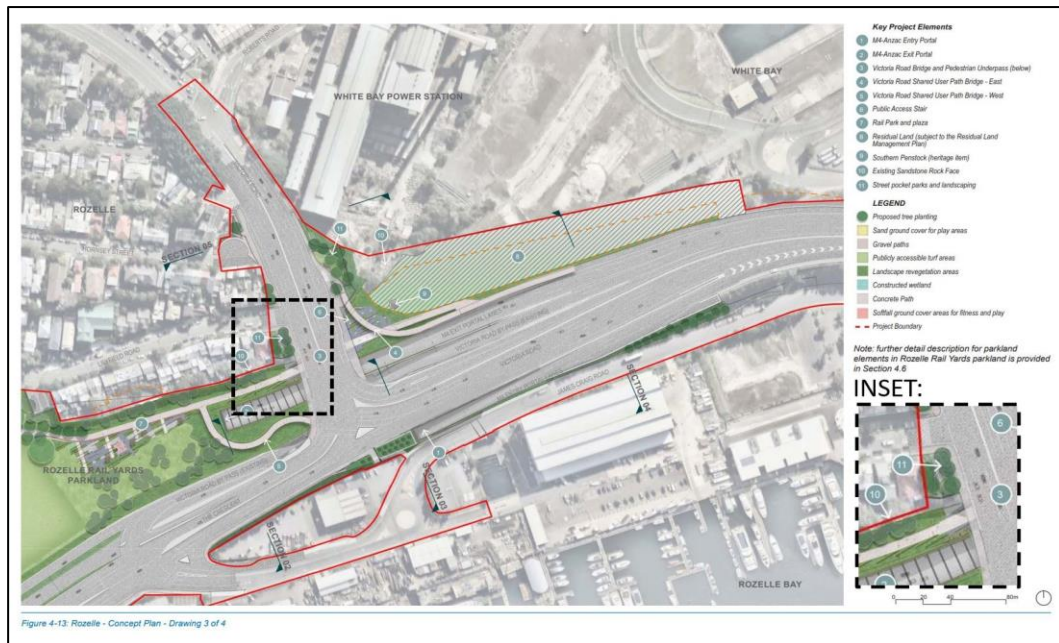
Above: The Motorway Facility to the west of the project is big enough to host a potential playing field and I will possibly sit below the cliff faces around it, so why not cover it over?

9. Is it possible to have the so-called 'void' depicted in Figure 4-17: Rozelle - Typical section 03 (p4-21), above the westbound tunnel cavity which is to pass from west of the Anzac Bridge, under the CityWest Link (westbound) and then into the terrain of the rail parkland above (to Iron Cove), to serve as a potential new pedestrian tunnel linking the Parkland in the Rail Park directly with Rozelle Bay foreshore? This would potentially obviate any need to cross the CityWest Link at traffic lights where it intersects with Victoria Road (a very unpleasant experience for peds and cyclists). The void depicted (according to the legend) certainly appears to seem high enough from the cross-section shown to host human pedestrians, and if not, could it be made so?



Above: This void could perhaps offer the key for connecting Rozelle Bay with the Rail Yard Park on the other side of the CityWest Link.

10. In "Figure 4-13: Rozelle - Concept Plan - Drawing 3 of 4" (shown below), it would appear the 'pocket park' listed (ie. Point 11) at the eastern end of Lilyfield Road, this is a wasted opportunity.



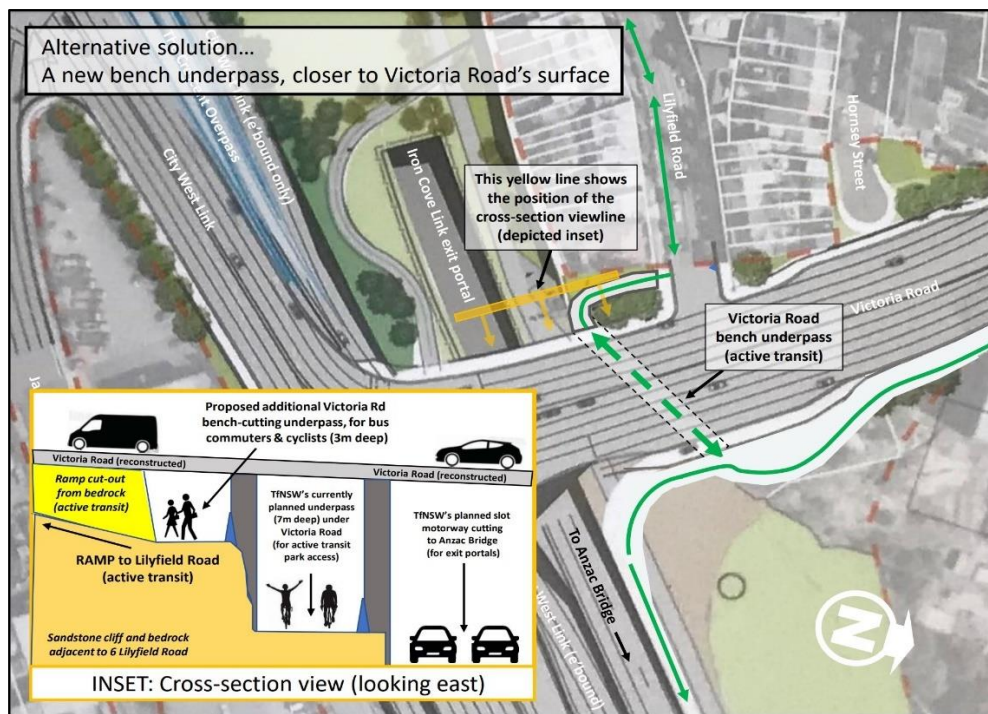
Above: Showing the position of the pocket park at the eastern end of Lilyfield Road that should offer a staircase down to Rail Yards Parkland to speed up the crossing of Victoria Road.

It truly beggars belief why the designers of the UDLP would not have grabbed the opportunity to at least provided a staircase or lift (or both!) at this location, down to the new Parklands from Victoria Road. It was at this southern end of Victoria Road where the former overpass once existed – which was incredibly important for local active movements for those living west of Victoria Road and the Anzac Bridge.

I know this better than most, because I once lived at 6 Lilyfield Road, the house immediately adjacent to this site. Why on earth would designers think to provide stairs on the eastern side of Victoria Road, but not on the western side? It's the western side, where the vast majority of residents wishing to cross to catch buses citybound live, so the change remains incomprehensible and will prove frustrating.

The terrain beneath your pocket park (Point 11) is solid sandstone – I know, once again I lived right next to it. To plant substantial trees or figs in that 'pocket park' would require some excavation and replacement of that sandstone with deep soil sinks – so I'm asking you to at least excavate some stairs as well, if that's what you're ultimately planning to do (like the UDLP suggests you will be doing on the eastern side of Victoria Road) to make a crossing of Victoria Road much faster for local residents who will be using it's bus services.

Ideally, you could've created a secondary bench underpass with ramps as part of the earth works around the new Victoria Road, by cutting a small amount of the existing cliff away (see below). This would have again made the crossing seem less 'out of the way' for vulnerable pedestrians wanting to cross Victoria Road, for that purpose alone – but at very least, stairs would make sense for the able-bodied users.



Above: Possible bench underpass and how it might work for faster Victoria Road crossings at White Bay.

11. Again, it beggars belief why the designers of this Draft UDLP might try so hard to suggest they've designed a 'green-link bridge' which 'improves active movements across the CityWest Link' for people in Rozelle, only to have its users then forced into crossing an amplified version of The Crescent or Johnson Street on the other side! The intersections where these pedestrians and cyclists must now cross over these roads should (in my opinion) also be provided with overhead crossings of their own – involving new shared-path bridges.

Such additions needn't be so-called 'green' or built at exorbitant in cost – they could be made affordable using lightweight but high-tech composite materials like those being manufactured in Queensland by Australian-owned Wagners¹. These could be custom designed and shipped to NSW by flatpack, before being assembled on-site and 'snapped-on' to side of existing structures.

For argument's sake, one could be snapped on to the southern side of the light rail bridge which lies east of The Crescent. In this way, a single design might cater for crossings of both Johnson Street and Chapman Road, allowing active users from all residential areas (ie. Annandale *and* Rozelle) to avoid traffic interactions altogether, across the amplified Crescent and Johnson Street.

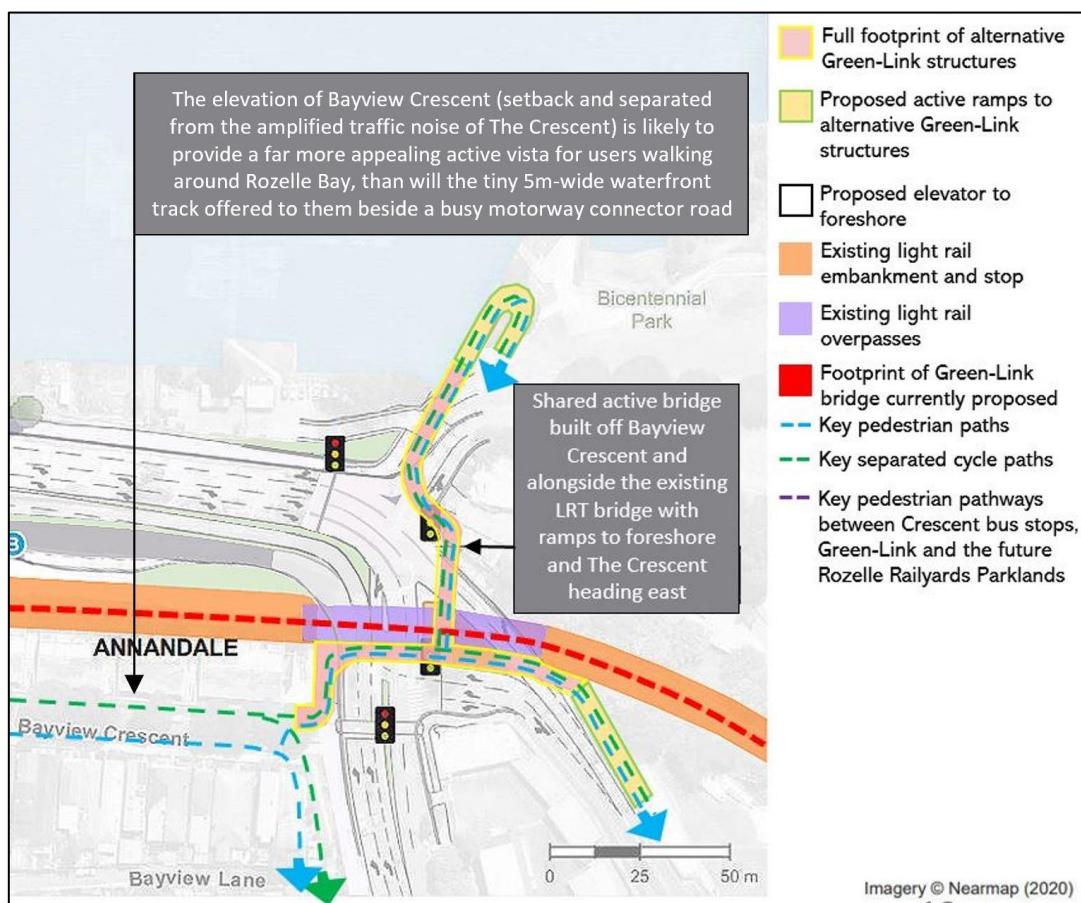
Such a bridge might ideally start at the eastern end of Bayview Road (which runs parallel to The Crescent but is already elevated. Bayview Road could become resident access only as far as parking is concerned – reducing movements and offering better vistas over The Bays and the City, compared to the lower and extremely narrow Foreshore Reserve walk currently being

¹ <https://www.wagner.com.au/main/what-we-do/composite-fibre-technologies/cft-home/>

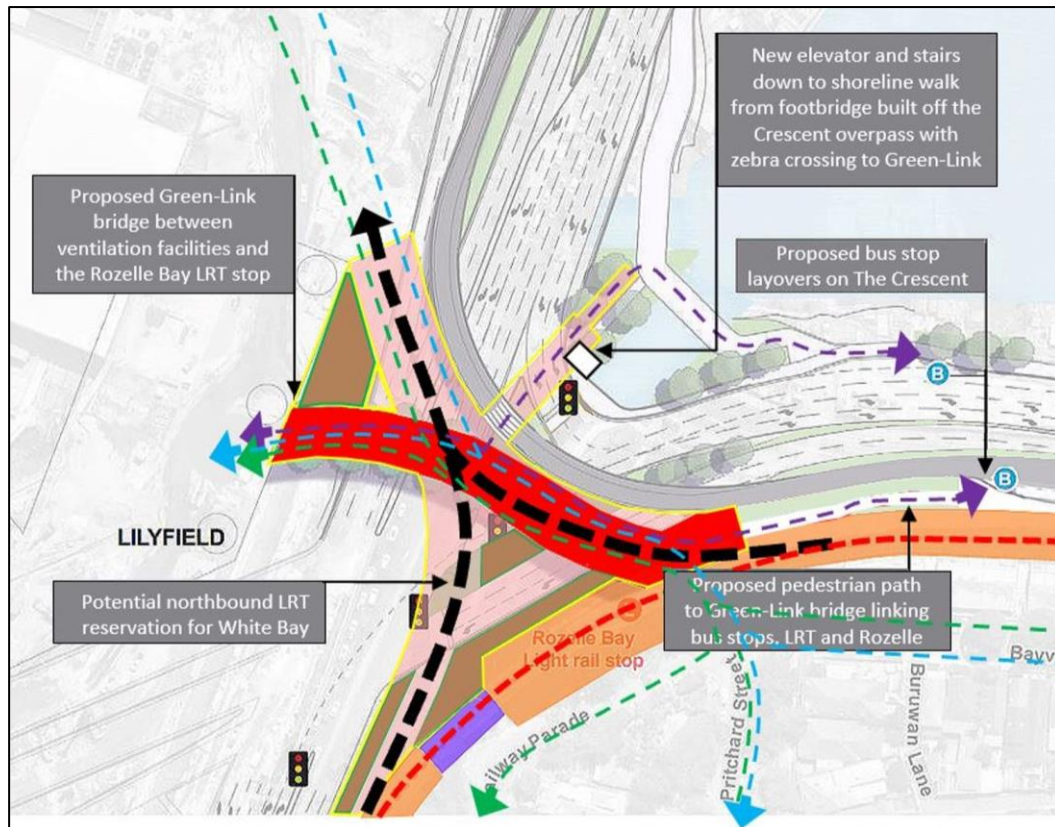
built at Rozelle Bay. The Foreshore Reserve is also likely not to be as pleasant a walk as Bayview Road, because it is so close in proximity to the intensified traffic noise of a widened Crescent.

The suggested 'snap-on bridge' could have ramps that head back to ground in an eastbound fashion, linking with Minogue Crescent on the other side of the rail bridge and the Bicentennial Parklands. It could also offer a level-crossing of the existing light rail bridge (much like those currently found at existing light rail stops). This would then allow for a crossing of Chapman Road as well, and the declining gradient down to the foreshore on the other side – again, this would be a liberating connection for users, free of all traffic (see image below).

A secondary bridge could also be built off the Crescent Overpass itself down to the Rozelle Bay foreshore, if combined with a raised zebra crossing (wombat crossing) over the carriageway, that could allow people to move directly from an expanded version of the Green-Link (built adjacent to the Crescent Overpass) to bridge safely from Rozelle to cross over safely to the Foreshore Park much more efficient.



Above: The Crescent/Johnson Street Overpass Suggestion



Above: A hypothetical Crescent pedestrian overpass to Rozelle Bay Foreshore (and surrounding links)

The combination of these bridge additions would allow for traffic-free and safe passage of peds or cyclists who wish to enter the Rozelle Bay Foreshore Park from the Green-Link – and would play a much stronger role in promoting active transport take-up by the local community around The Bays and Inner West region.

In Summary:

Thank you for the opportunity to comment on your draft UDLP. While there are opportunities to do many cosmetic things to existing draft design, there are two key additions which need to be made, and which hopefully this submission has brought to your attention, those being:

1. The need to reserve a transit corridor for a potential light rail link to White Bay for exchange with the Metro West and the surrounding suburbs in future.
2. Designs should incorporate this transit corridor now as a potential viaduct to improve not only public transport movements, but also active movement connections.

There are other changes I have suggested which could prove significant, like additional footbridges, opportune underpasses through pre-fab motorway voids and green rooves to maximise public space.

Ultimately, the outcome of this project and how well it is received by the community will depend on the thoughtfulness by which the designers put themselves in the users' shoes. How safe will I feel? What will I smell, what will I see - and most

overlooked, but probably the most important when it comes to attracting users, how much noise pollution is likely to be forced upon me?

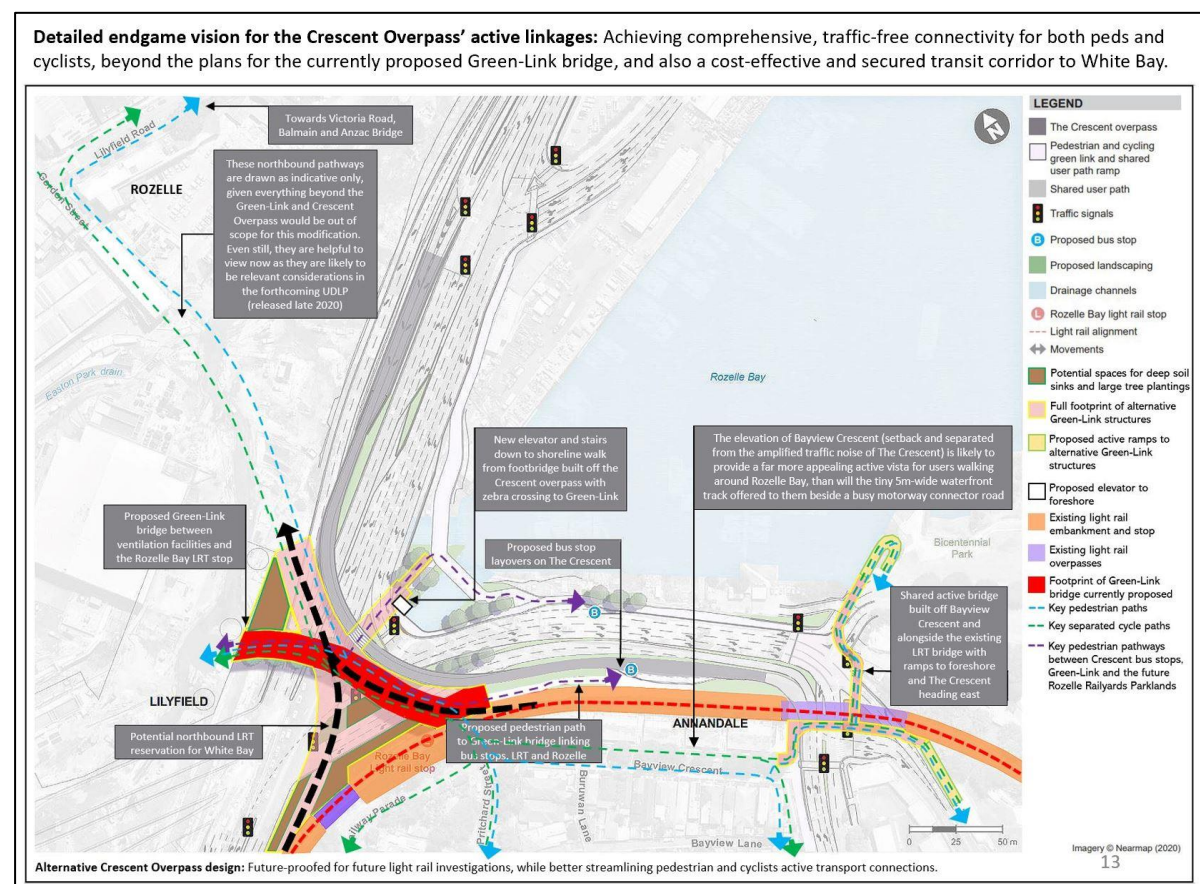
Empathy is everything, and the pedestrian is the most sensitive to all these things.

Kind regards,



Nathan English,
MPlan, GradCert (Jour) & BComms (Media)
Resident of Balmain East,
[REDACTED]

Email: [REDACTED]



Bays West Structure Plan 2040 and beyond

LEGEND

- Bays West Site Boundary
- Light Rail Station
- Light Rail Route
- Future 'The Bays' Metro Station
- Future 'The Bays' Metro Station Box
- Proposed Active Transport Connection
- Potential Future Active Transport Connection
- Proposed Bus Stops/ Interchange
- Key Heritage Landmarks
- Proposed Key Public Domain
- Key Landform
- Foreshore Promenade
- Proposed Promenade Linking Connections
- Occasional Foreshore Promenade Access (non-ship days)
- Existing Foreshore Promenade
- Proposed Zone of Development
- Proposed Taller Building Cluster
- Integrated Development/ Ports & Working Harbour
- Public Domain integrated into projects
- Integrated Ports Facility with Elevated Public Domain
- Ports & Working Harbour Zone
- Vessel Berthing Zone



Courtesy of Terroir and collaborators.